

WELCOME!

Public Information Centre #2

**Hawkesbury Bridge Replacements (GWP 4203-15-00)
Transportation Environmental Study Report Review and Addendum
October 17, 2024 @ 4:00 PM – 7:00 PM**



Project Description

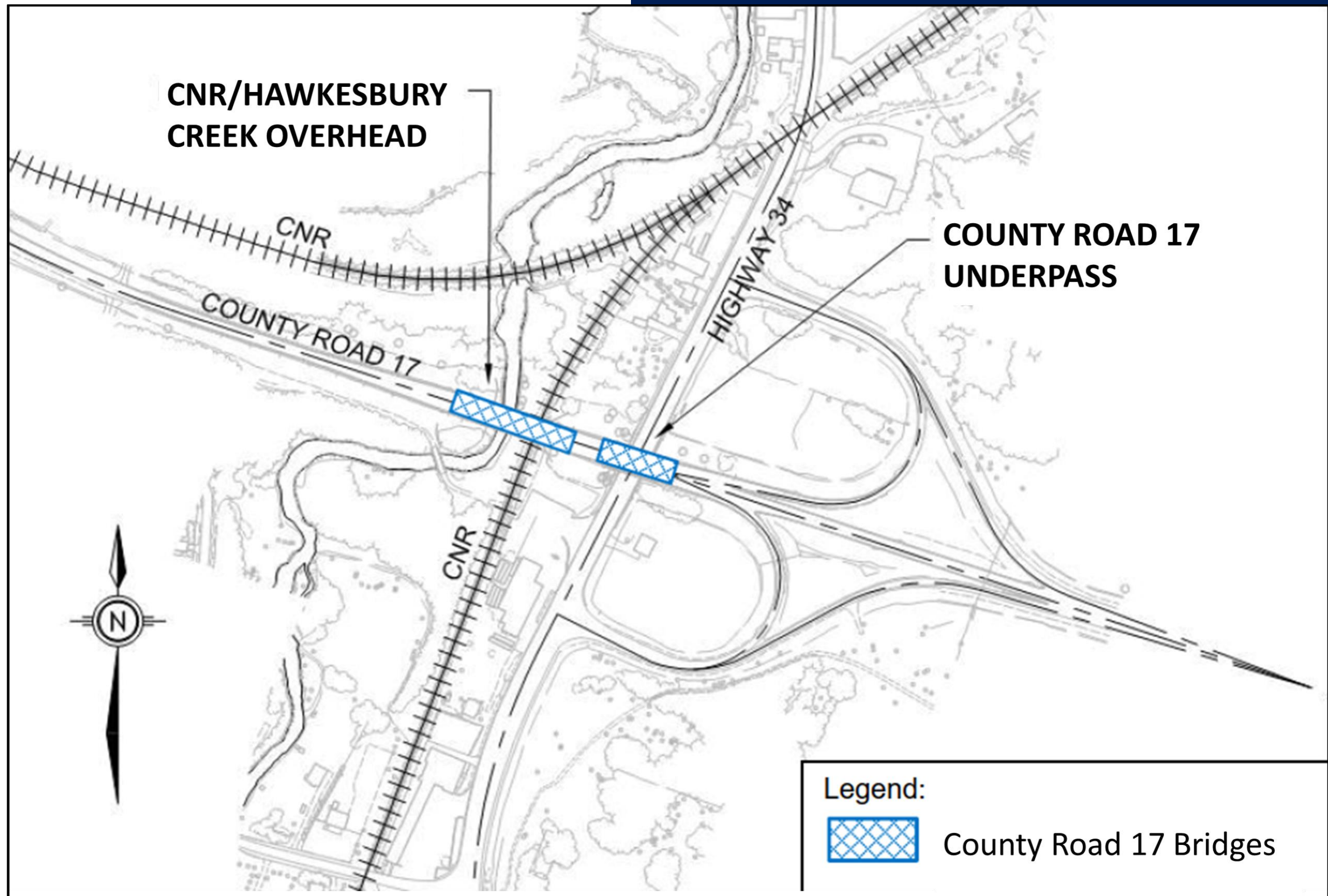
The Ministry of Transportation Ontario (MTO) has retained Dillon Consulting and Jacobs to complete the Preliminary Design and Transportation Environmental Study Report (TESR) for the replacement of the following two bridges:

- CNR/Hawkesbury Creek Overhead
- County Road 17 Underpass (at Highway 34)

Both bridges are nearing the end of their useful service life and require replacement. Major components of the Project includes:

- Replacement of the two bridges;
- Modifications to the east bound off-ramp and west bound on-ramp at the County Road 17 and Highway 34 interchange; and
- Temporary road closures of Highway 34 and County Road 17 during construction during which traffic will be detoured

Construction is anticipated to be initiated in 2027 subject to approvals and funding.



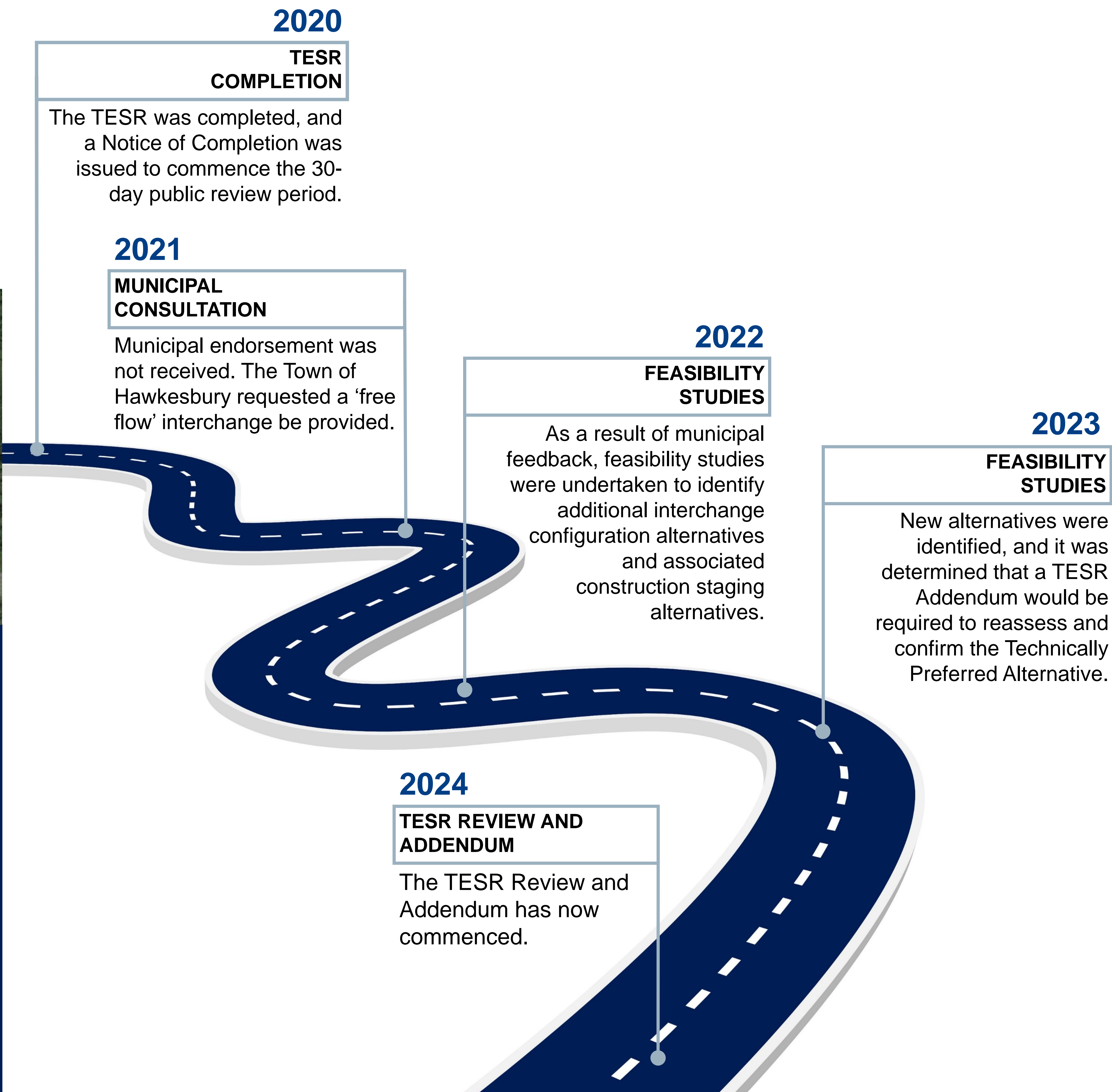
Project Background



In November 2020, the TESR for the Hawkesbury Bridge Replacements Class Environmental Assessment Study was completed.

Municipal endorsement of the Technically Preferred Alternative Method for the Project, outlined in the TESR, was not received.

Between 2021 and 2023, additional municipal consultation and feasibility studies were completed. Based on this work, it was determined that a TESR Review and Addendum would be required.



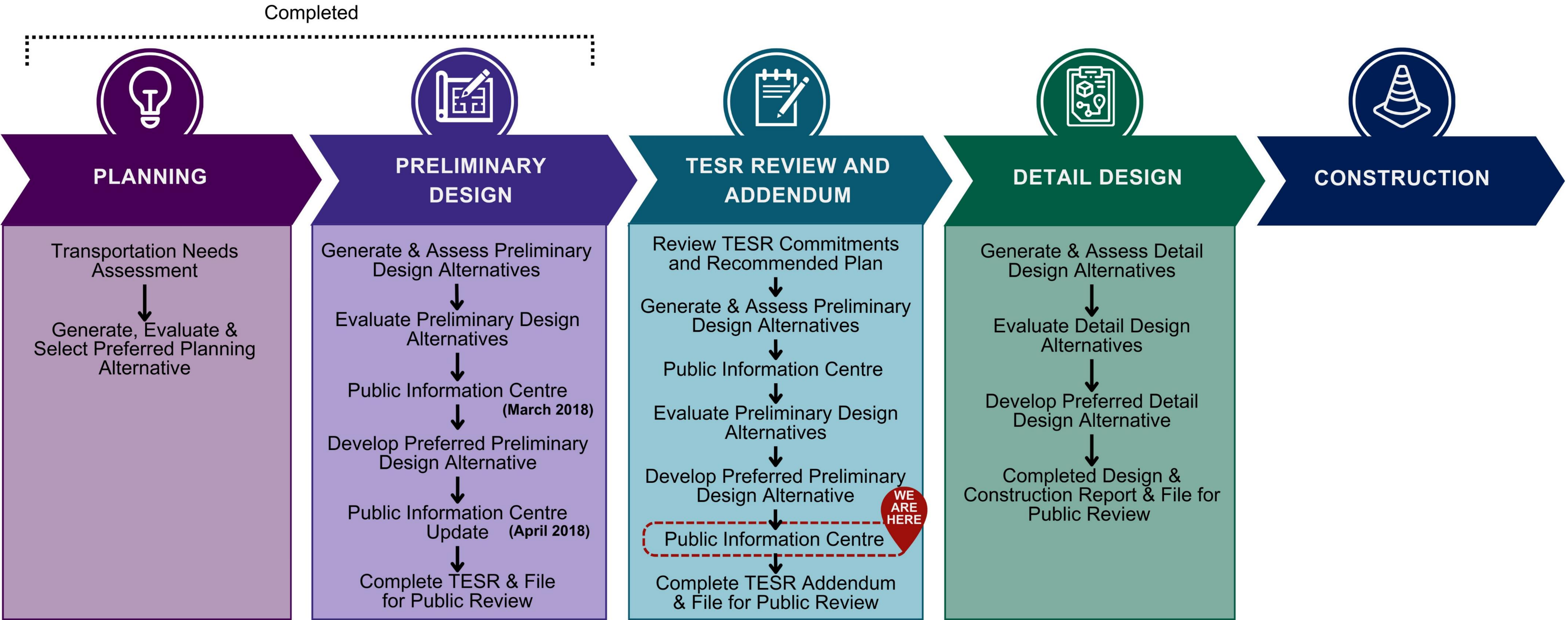
Environmental Assessment Process

The Project is following the process for Group B undertakings in accordance with MTO’s *Class Environmental Assessment for Provincial Transportation Facilities*, 2000 (Class EA).

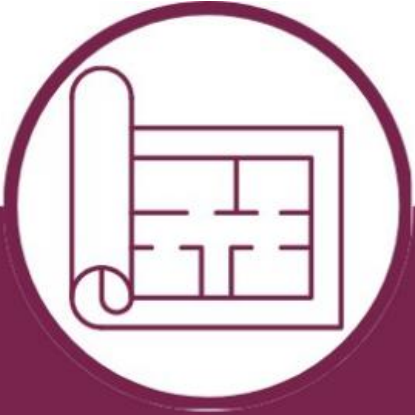






In November 2020, the Class EA Study for the Project was completed. A Transportation Environmental Study Report (TESR) was posted for public review, documenting a Technically Preferred Alternative Method for the Project.

Any significant modification to the Project that occurs after completion of the TESR must be reviewed and a TESR Addendum must be prepared. Once complete, the TESR Addendum will be available on the project website (www.cr17bridges.com).

CLASS EA PROCESS



2020 TESR Recommended Plan Features – Currently Under Review

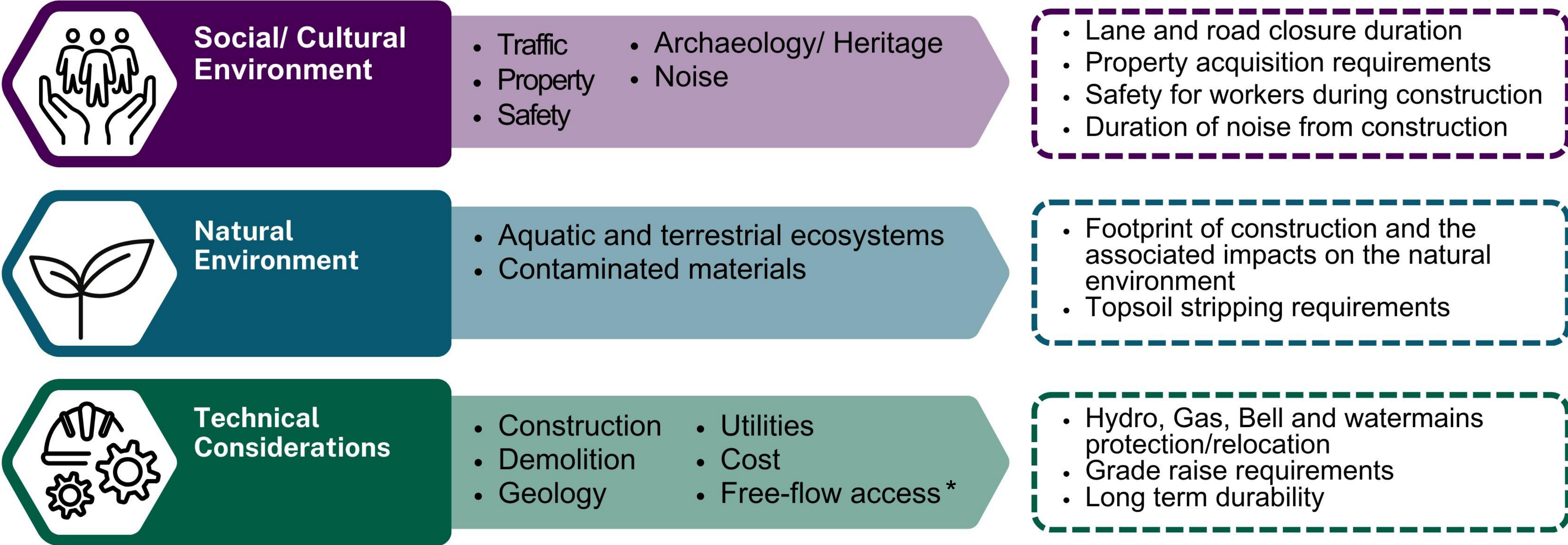
TESR RECOMMENDED PLAN FEATURE	 BRIDGE LAYOUT	 HORIZONTAL AND VERTICAL ALIGNMENTS	 CROSS-SECTION	 INTERCHANGE MODIFICATIONS	 UTILITIES	 PROPERTY	 CONSTRUCTION TRAFFIC MANAGEMENT
SUMMARY OF TESR RECOMMENDATION	<ul style="list-style-type: none">The new CNR/Hawkesbury Creek Overhead will be a 71 m long, 13 m wide, two-span concrete slab-on-steel I-girder bridge.The new CR 17 Underpass will be a 36 m long, 13 m wide, single span concrete slab-on-steel I-girder bridge.	<ul style="list-style-type: none">The existing alignment of CR 17 will be maintained.The replacement bridges will be raised in order to provide the minimum vertical clearance over the CNR tracks and over Highway 34.	<ul style="list-style-type: none">North Barrier Wall: 0.5 mShoulder: 2.5 mTraffic Lanes: two @ 3.5 mShoulder: 2.5 mSouth Barrier Wall: 0.5 mTotal width: 13 m	<ul style="list-style-type: none">New intersection of CR 17 and the Highway 34 on-ramp and off-ramp.Off-ramp facilitated via a channelized right turn including a right turn lane.On-ramp facilitated by a stop control T-Intersection, and no acceleration lane provided.	<ul style="list-style-type: none">Protection of Enbridge gas main, Hydro One underground conduits, and watermain during construction.Relocation of underground Bell duct.Removal and temporary replacement of guy wire supporting hydro pole; reinstated to existing conditions following construction.	<ul style="list-style-type: none">A standard grade separation reconstruction agreement between CNR and MTO in accordance with the <i>Railway Safety Act</i> is required to replace the existing CNR/Hawkesbury Creek Overhead bridge.	<ul style="list-style-type: none">Closure of CR 17 for 2-4 weeks: traffic detoured along CR 4 heading E-W through Hawkesbury Main St and heading N-S along Tupper St.Closure of Highway 34 near the site for 2-3 weekends: traffic detoured to a temporary bypass east of Highway 34 to maintain N-S traffic movements along Highway 34.
RECOMMENDED PLAN IMPACTED BY TESR ADDENDUM?	Alternatives under review and assessment.	To be confirmed based on alternatives assessment.	Alternatives under review and assessment.	Alternatives under review and assessment.	To be confirmed based on alternatives assessment.	To be confirmed based on alternatives assessment.	Alternatives under review and assessment.

Alternatives Evaluation Criteria

A reason-based decision-making process was applied to identify and determine the Technically Preferred Alternative(s) based on an assessment of the advantages, disadvantages, and associated trade-offs under each of the following criterion:

Interchange Configuration alternatives, and Construction Staging alternatives were evaluated to select a new Technically Preferred Alternative for the Project. For consistency, the evaluation followed the same methodology and used the same criteria as was used in the 2020 TESR.

CRITERIA



* New criterion based on municipal feedback.

Summary Evaluation of Interchange Configuration Alternatives

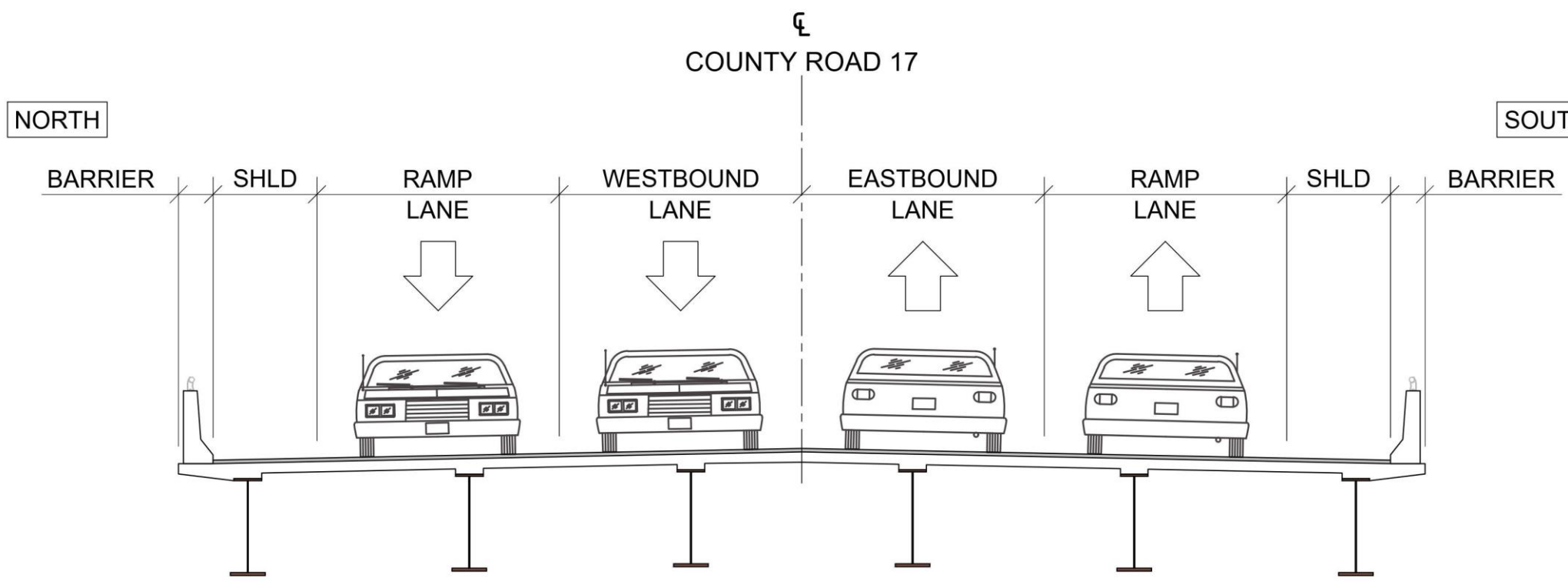
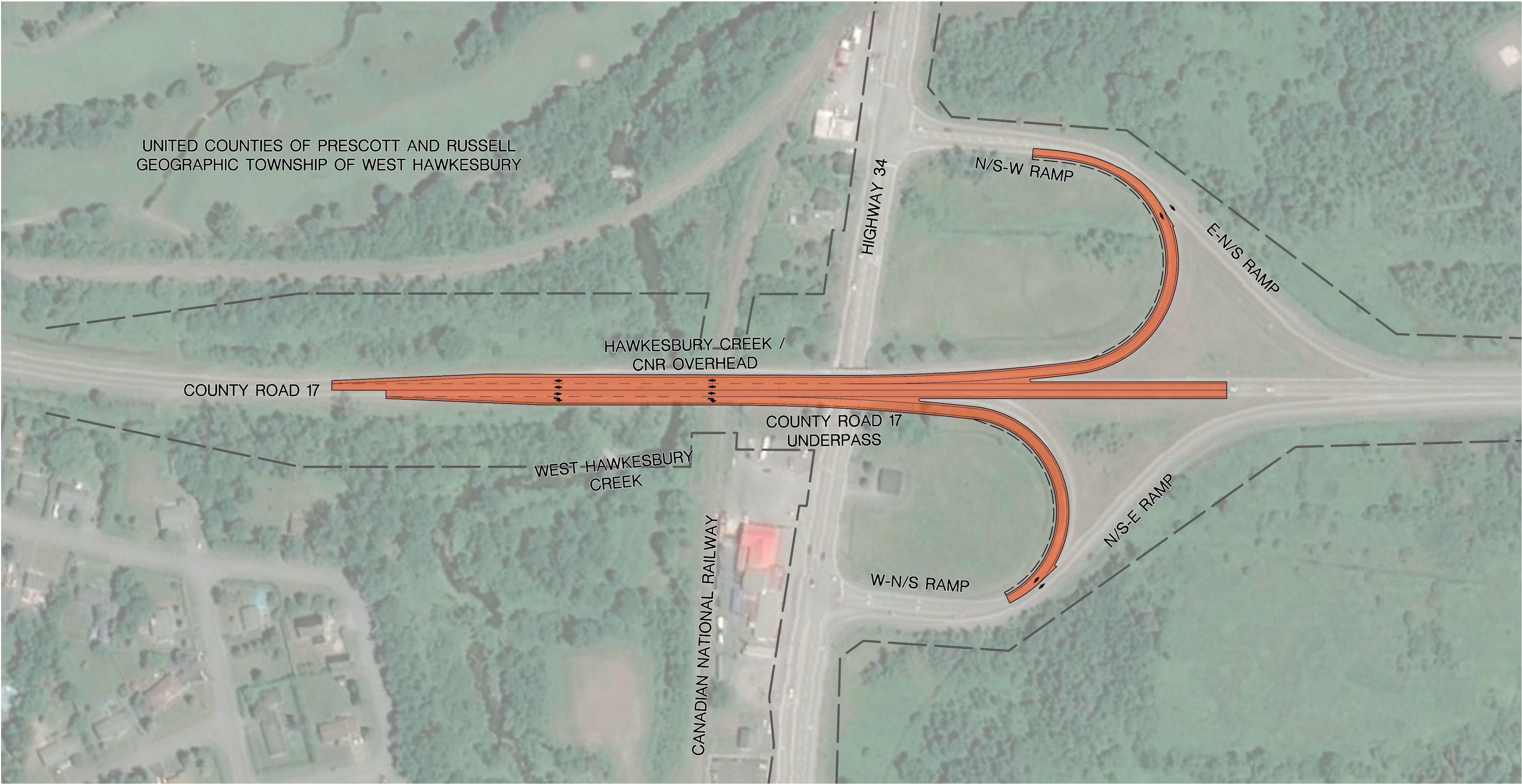
Impacts	Alternative 3 Free-flow access for N/S-W movement; deceleration lane and low-speed right turn for W-N/S movement.	Alternative 4 Like for like replacement of the existing interchange.
Social/Cultural Environment	<ul style="list-style-type: none">Operates at a similar level of service compared to existing conditions. ✓Free-flow movements are not warranted based on highway classification and observed traffic volumes. ✗Additional property is not required. ✓No impacts to archaeology and built heritage. ✓No change in noise impacts compared to existing conditions. =Supported but not preferred by the public and Town/Township/County. ✓	<ul style="list-style-type: none">Operates at a similar level of service compared to existing conditions ✓Free-flow movements are not warranted based on highway classification and observed traffic volumes. ✗Additional property is not required. ✓No impacts to archaeology and built heritage. ✓No change in noise impacts compared to existing conditions. =Preferred by the public and Town/Township/County. ✓
Natural Environment	<ul style="list-style-type: none">No anticipated impacts to Species at Risk (SAR) habitat. ✓	<ul style="list-style-type: none">No anticipated impacts to SAR habitat. ✓
Technical Considerations	<ul style="list-style-type: none">Not a standard interchange design in Ontario and is inconsistent with driver expectations. ✗Meets design criteria. ✓Potential for wrong way left turn movements from County Rd 17 onto the W-N/S ramp. ✗	<ul style="list-style-type: none">Is a standard interchange design in Ontario and is consistent with driver expectations. ✓W-N/S ramp does not meet current design standards. ✗No potential for wrong way left turn movements from County Rd 17 onto the W-N/S ramp. ✓
Technically Preferred Alternative	Not Preferred	Preferred

Ramp Configuration Alternative 4

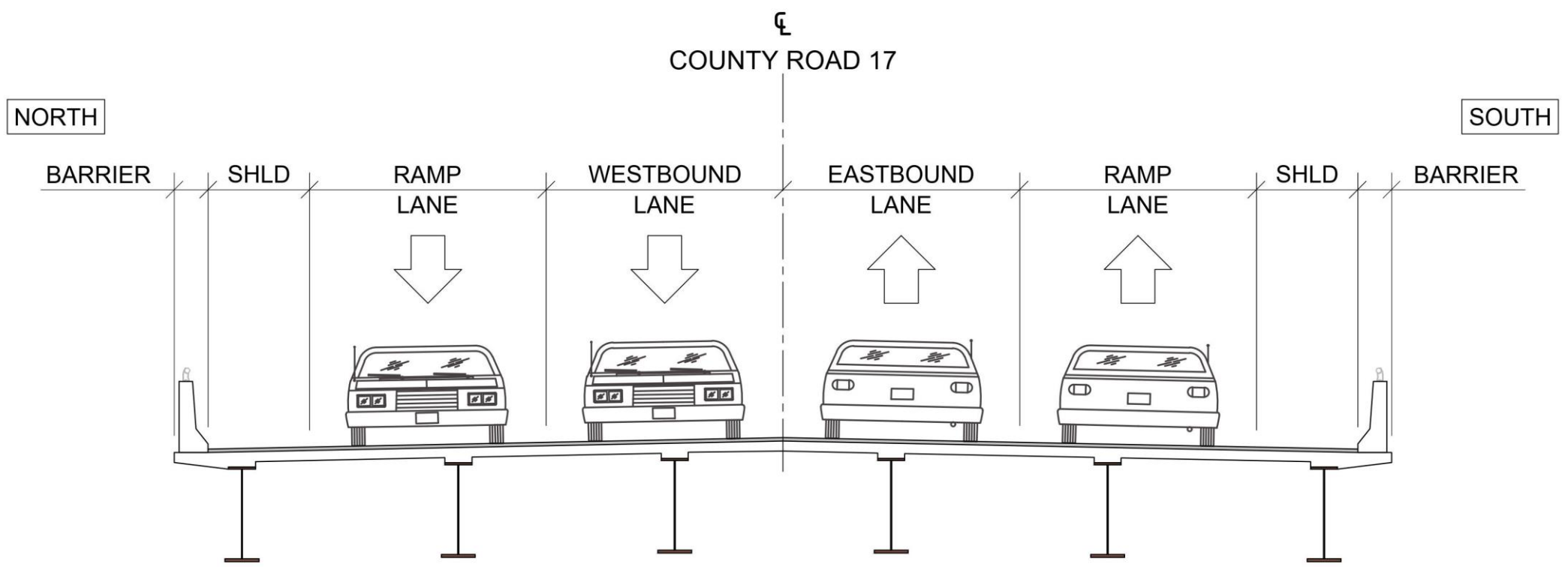
Selected as the Technically Preferred Alternative

This alternative was selected for the following key reasons:

- Maintains free flow traffic for all ramps
- Does not impact private property
- Standard interchange configuration in Ontario
- Similar traffic capacity to the existing interchange configuration
- No opportunities for left-turn movements from County Road 17
- Interchange similar to the existing configuration
- There is limited history of incidents with the small radius W-N/S exit ramp.



Hawkesbury Creek/CNR Overhead



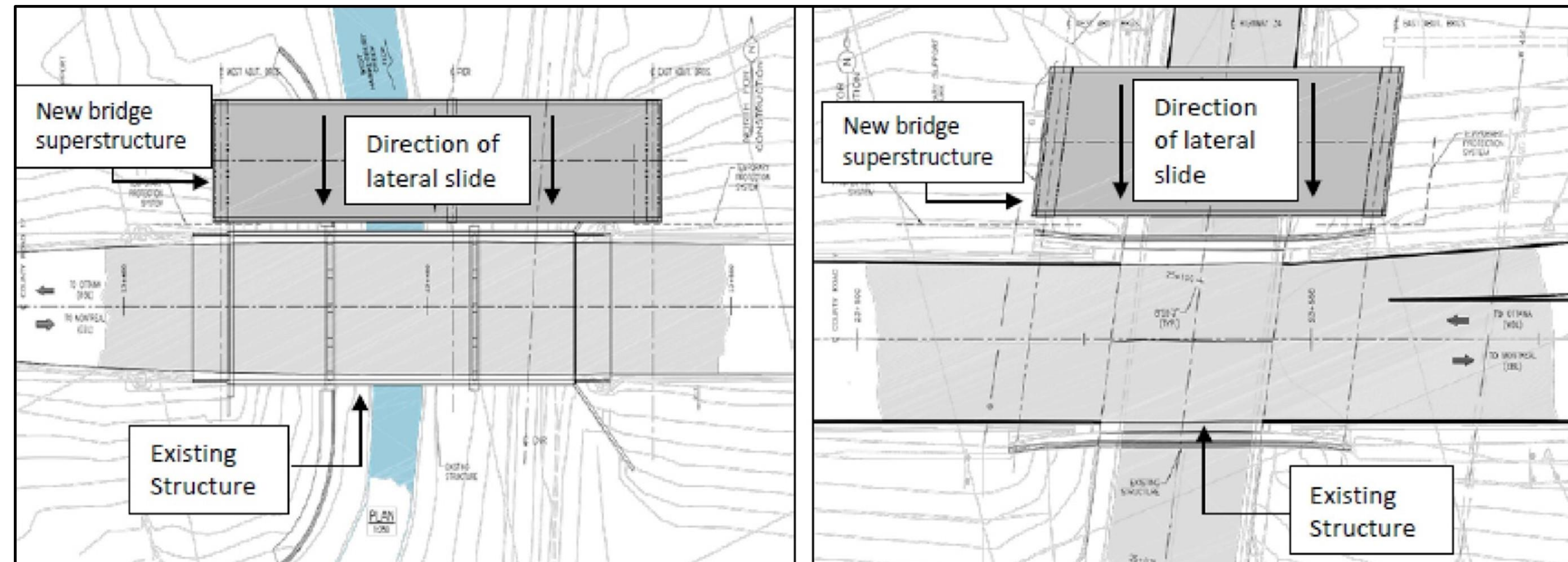
County Road 17 Underpass

Staging Alternative 1

Selected as the Technically Preferred Alternative

This staging method was selected for the following key reasons:

- Maintains acceleration and deceleration lanes during the winter shutdown period
- Maintains 3.5 m lanes and full shoulders during the winter shutdown period
- Has a shorter construction duration
- Maintains two lanes of traffic on County Road 17, except during full closures
- Does not require temporary support of the existing structure



County Road 17 Closure

To accommodate the rapid demolition and lateral slide, County Road 17 will be closed completely for 3 to 5 weeks.

Highway 34 Closure

A minimum of three overnight closures of Highway 34 are required for three weekends to erect the new girders, demolish the existing structure, and jack and slide the new structure into place.

Summary Evaluation of Detour Route Alternatives

Impacts	Option 1 Tupper Street/Main Street Detour Route	Option 2 Highway 34/McGill Street Detour Route
Social/Cultural Environment	<ul style="list-style-type: none">Anticipated 8 min out of way travel time. X	<ul style="list-style-type: none">Anticipated 7 min out of way travel time. ✓
Natural Environment	<ul style="list-style-type: none">No anticipated impacts to Species at Risk (SAR) habitat. ✓	<ul style="list-style-type: none">No anticipated impacts to SAR habitat. ✓
Technical Considerations	<ul style="list-style-type: none">Main Street East is impacted by the detour route. XRequires a portion of parking to be removed on Main Street East to accommodate trucks. XRequires temporary signals at Tupper Street/Main Street intersection and modifications to four existing intersections (only applicable to Lateral Slide construction staging alternative). X	<ul style="list-style-type: none">Main Street East is not impacted by the detour route. ✓No impacts to parking on Main Street East. ✓No additional roadway improvements required beyond existing municipal plans. ✓
Technically Preferred Alternative	Not Preferred	Preferred

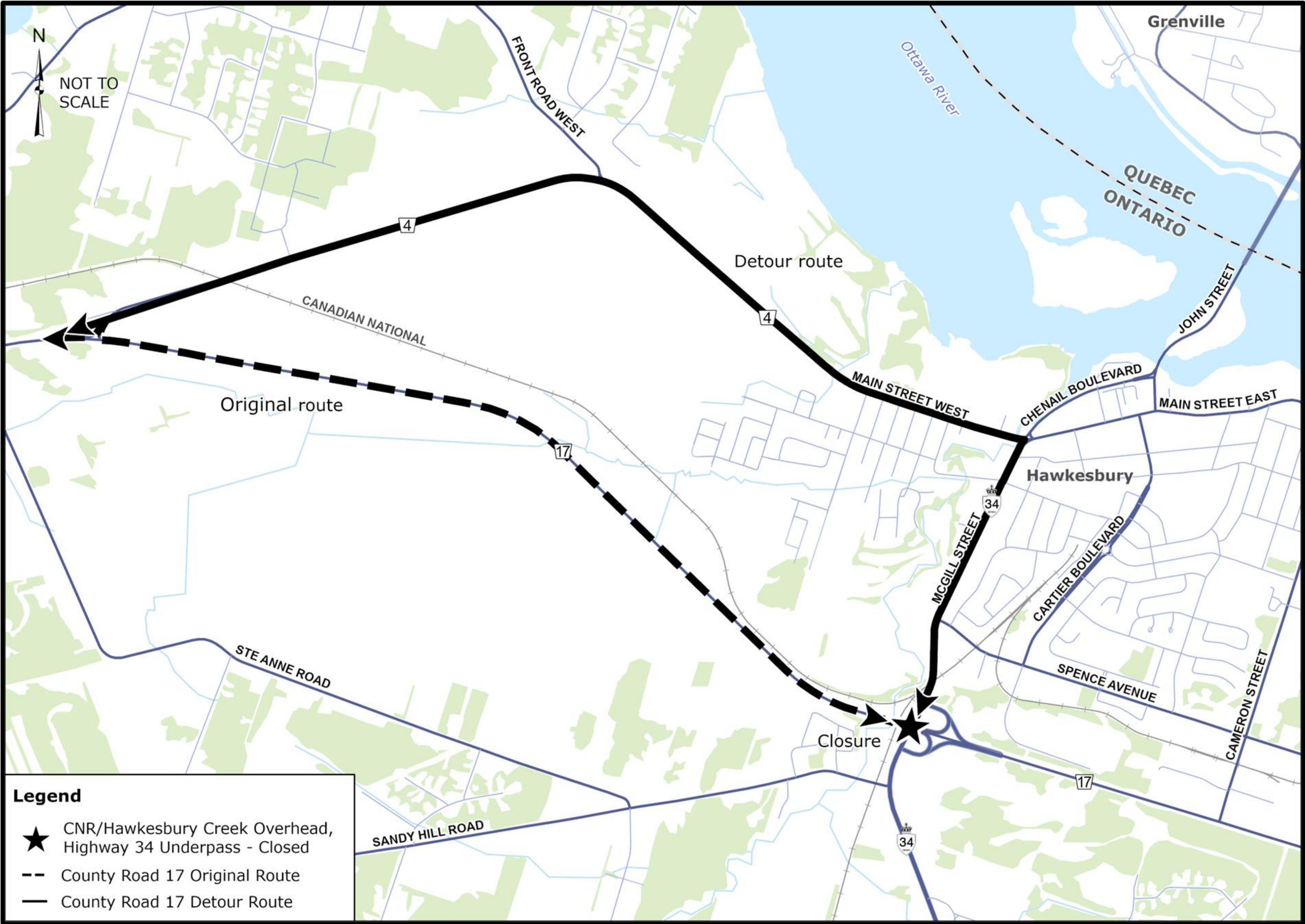
Note:

County Road 17 will require a detour of 3-5 weeks for the Lateral Slide construction staging alternative.

County Road 17 Detour Route Option 2

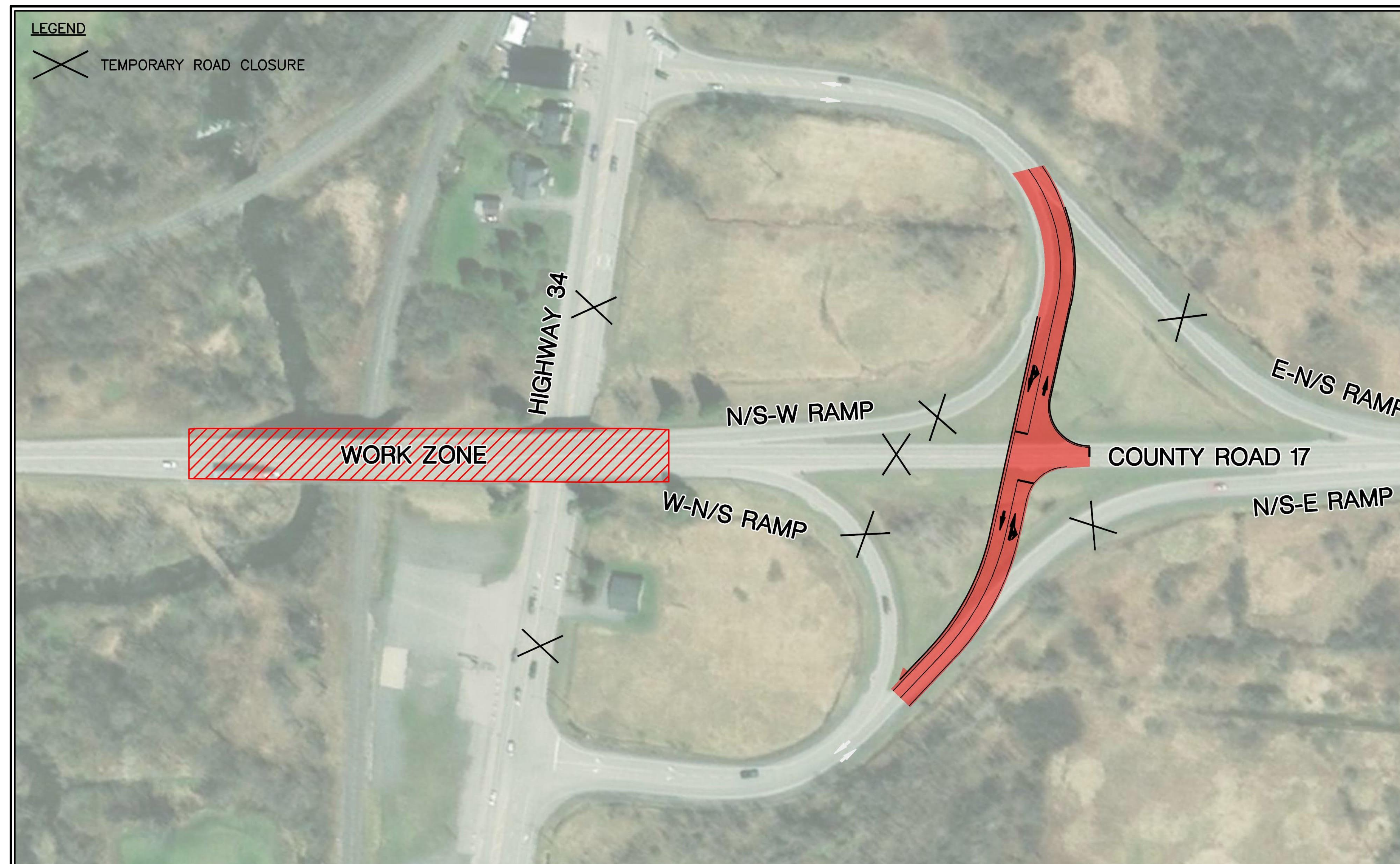
Selected as the Technically Preferred Alternative

- This detour was selected for the following key reasons:
- It has the shortest anticipated out of way travel time
 - Does not impact Main Street East
 - It does not impact parking on Main Street East
 - No additional roadway improvements required beyond existing municipal plans




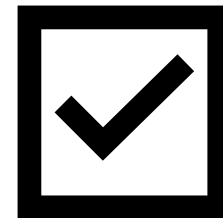

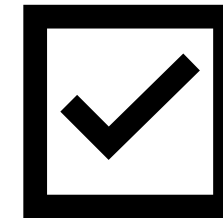

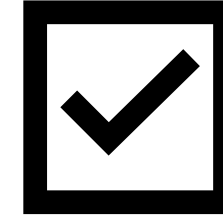
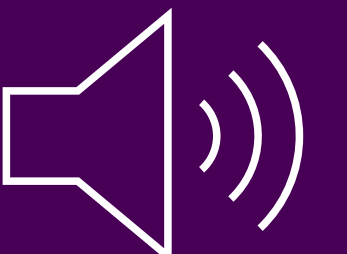
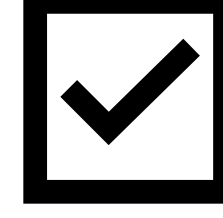

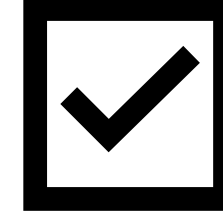
County Road 17 traffic will be detoured for 3-5 weeks along Main Street heading north-south along McGill Street.

Highway 34 Detour Route



Highway 34 traffic will be detoured via a temporary north-south bypass through the interchange. This detour is required for approximately three overnight closures while Highway 34 is closed.

Environmental Impacts and Mitigation

Environment/ Element	Potential Impacts	Change from the TESR Recommended Plan	Mitigated by Standard Mitigation Measures?
<div>Vegetation and Wildlife</div> <div></div>	<ul style="list-style-type: none">Temporary loss and/or disruption to wildlife and/or wildlife habitat, including migratory birds and Species at Risk (SAR)	<ul style="list-style-type: none">Slight change in area of disturbed vegetationBlack Ash have potential to occur in the Study Area and have now been listed as a SAR	<div></div>
<div>Fish and Fish Habitat</div> <div></div>	<ul style="list-style-type: none">Mortality, entrapment or entrainment of fishContamination of the watercourseLoss and/or disruption of aquatic habitat	<ul style="list-style-type: none">No change.	<div></div>
<div>Archaeology</div> <div></div>	<ul style="list-style-type: none">Potential to encounter previously undocumented archaeological resources during construction	<ul style="list-style-type: none">No change.	<div></div>
<div>Noise</div> <div></div>	<ul style="list-style-type: none">Temporary disturbance to nearby residents and businesses during construction	<ul style="list-style-type: none">No change.	<div></div>
<div>Traffic</div> <div></div>	<ul style="list-style-type: none">Lane and road closures will cause temporary disruptions to traffic	<ul style="list-style-type: none">Full closure of CR 17 for 3-5 weeksMinimum of 3 overnight closures of Highway 34	<div></div>

Next Steps

We welcome your feedback on the information provided.

Following this PIC, the Project Team will:



1. Review all comments received from stakeholders and the public.



2. Confirm the Recommended Plan based on comments received.



3. Prepare the TESR Addendum.



4. File the TESR Addendum on the project website (www.cr17bridges.ca) for a 30-day public review period.

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